



# ST. CHARLES PARISH


## OFFICE OF THE COUNCIL SECRETARY

P.O. BOX 302 • HAHNVILLE, LOUISIANA 70057  
(985) 783-5000 • www.stcharlesparish.gov

### COUNCIL OFFICE MEMORANDUM

DATE: JANUARY 10, 2025

TO: MR. MILES BINGHAM  
PUBLIC WORKS DIRECTOR

FROM: MICHELLE IMPASTATO   
COUNCIL SECRETARY

RE: CHANGE ORDER NO. 2  
CONCRETE MAINTENANCE 2023-24  
(PROJECT NO. P230803)

On January 6, 2025, the St. Charles Parish Council adopted Ordinance No. 25-1-3 approving and authorizing the execution of Change Order No. 2 with Kass Bros., Inc., for Concrete Maintenance 2023-24 (Project No. P230803), to increase the contract amount by \$328,314.80 and to increase the contract time by sixty (60) days.

A fully executed ordinance and change order have been recorded. A copy of the recorded ordinance and two (2) original change orders are enclosed for your records.

MI/ag

Enclosures

cc: 1Parish Council  
Ms. Samantha de Castro w/enclosure  
Mr. Corey Oubre w/enclosure  
Mr. Grant Dussom w/enclosure  
Mr. Brandon Bernard w/enclosure  
Digital Engineering and Imaging, Inc. w/original enclosure  
Kass Bros., Inc. w/original enclosure

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<sup>1</sup> Council Dept. retained 1 original for file; 1 original filed in COC (2)

2024-0422

INTRODUCED BY: MATTHEW JEWELL, PARISH PRESIDENT  
(DEPARTMENT OF PUBLIC WORKS)

ORDINANCE NO. 25-1-3

An ordinance approving and authorizing the execution of Change Order No. 2 with Kass Bros., Inc., for Concrete Maintenance 2023-24 (Project No. P230803), to increase the contract amount by \$328,314.80 and to increase the contract time by sixty (60) days.

**WHEREAS,** Ordinance No. 22-7-2 adopted on July 5, 2022, by the St. Charles Parish Council, approved and authorized the execution of a Professional Services Agreement with Digital Engineering & Imaging, Inc., to perform planning services for the Road Maintenance 2022-23 (Project No. P220501), in the not to exceed amount of \$316,728.72; and,

**WHEREAS,** Ordinance No. 23-9-2 adopted on September 25, 2023, by the St. Charles Parish Council, approved and authorized the execution of Amendment No. 1 to Ordinance No. 22-7-2, which approved the Professional Services Agreement with Digital Engineering & Imaging, Inc. to perform planning services for the Road Maintenance 2022-23 (Project No. P220501), adding Concrete Maintenance 2023-24 (Project No. P230803), in the amount of \$211,740.00; and,

**WHEREAS,** Ordinance No. 24-7-19 adopted on July 22, 2024, by the St. Charles Parish Council, approved and authorized the execution of a Contract with Kass Bros., Inc., for Concrete Maintenance 2023-24 (Project No. P230803), in the amount of \$914,593.10; and,

**WHEREAS,** Ordinance No. 24-12-4 adopted on December 2, 2024, by the St. Charles Parish Council, approved and authorized the execution of Change Order No. 1 with Kass Bros., Inc., for Concrete Maintenance 2023-24 (Project No. P230803), to increase the contract amount by \$193,430.85 and increase the contract time by thirty-five (35) days; and,

**WHEREAS,** it is necessary to amend the contract and incorporate additional quantities added during construction that improved the quality of the roadway and to add the maintenance work on Ares Street, Scorpio Street, and Leo Street; and,

**WHEREAS,** St. Charles Parish and Kass Bros., Inc., have mutually agreed to increase the contract amount by \$328,314.80 and increase the contract time by (60) calendar days to complete the work.

**THE ST. CHARLES PARISH COUNCIL HEREBY ORDAINS:**

**SECTION I.** That Change Order No. 2 for Concrete Maintenance 2023-24 (Project No. P230803), to increase the contract amount by \$328,314.80 and to increase the contract time by sixty (60) days is hereby approved and accepted.

**SECTION II.** That the Parish President is hereby authorized to execute said Change Order on behalf of St. Charles Parish.

The foregoing ordinance having been submitted to a vote, the vote thereon was as follows:

YEAS: MOBLEY, FONSECA, WILSON, SKIBA, PILIE, COMARDELLE,  
O'DANIELS, FISHER, DEBRULER  
NAYS: NONE  
ABSENT: NONE

And the ordinance was declared adopted this 6th day of January, 2025, to become effective five (5) days after publication in the Official Journal.

CHAIRMAN: Nelly Fonseca  
SECRETARY: Michelle Dupontato  
DLVD/PARISH PRESIDENT: January 7, 2025  
APPROVED: ✓ DISAPPROVED: \_\_\_\_\_  
PARISH PRESIDENT: Matthew Jewell  
RETD/SECRETARY: January 8, 2025  
AT: 9:25am RECD BY: (Signature)

RECORDED IN THE ST. CHARLES PARISH  
CLERK OF COURT OFFICE  
ON 1/8/25  
AS INSTRUMENT NO. 486228  
IN (MORTGAGE) CONVEYANCE/OATH BOOK

ST. CHARLES PARISH  
CLERK OF COURT  
RECEIVED  
JAN 10 2025

CHANGE ORDER

No. 02

DATE OF ISSUANCE 12/11/24

EFFECTIVE DATE 12/11/24

OWNER: St Charles Parish
CONTRACTOR: Kass Bros., Inc.
CONTRACT: Concrete Maintenance 2023-24
PROJECT: Concrete Maintenance 2023-24
OWNER's Contract No. P230803 ENGINEER's Contract No. P230803
ENGINEER Digital Engineering & Imaging, Inc.

You are directed to make the following changes in the Contract Documents:

Description:

- 1. Add the Following Work Items:
a. # X004: 10" PCCP

Addition of \$ 241,372.80 SY(LF). 1734 SY @ \$139.20/SY.

Total of Added Work Items = (+ \$241,372.80)

- 2. Revise the Following Work Item Quantities:

- a. # 020: REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT
Addition of \$ 38,692.00 (SY). 2276 SY @ \$17.00/SY
b. # 030: PORTLAND CEMENT CONCRETE PAVEMENT

Addition of \$ 48,250.00 (SY). 500 SY @ \$96.50/SY.

Total of Change in Work Items Quantity = (+ \$86,942.00)

Reason for Change Order:

- 1. Add Work Items
a. Additional Concrete Roads being added to the Maintenance contract with 10" panels to increase the road strength for heavy traffic in Industrial area (Leo, Aries, and Scorpio)
2. Revise Work Item Quantities
a. The existing contract item (020) is being increased in quantities for the removal of PCCP on Post St that were not included in the original assessment and includes the removal of PCCP for Leo, Aries, and Scorpio (1,734 SY). These additional quantities were assessed and agreed upon
b. The existing contract item (030) is being increased in quantities for the additional panels on Post St that were not included in the original assessment. These additional quantities were assessed and agreed upon

Attachments: FC04, 10" Typical Details, & Engineer's Opinion of Probable Cost

| CHANGE IN CONTRACT PRICE:   |
|---|
| Original Contract Price<br>\$ <u>914,593.10</u>   |
| Net Increase (Decrease) from previous Change Orders<br>No. <u>1</u> to <u>2</u> :<br>\$ <u>193,430.85</u> |
| Contract Price prior to this Change Order:<br>\$ <u>1,108,023.95</u>                                      |
| Net increase (decrease) of this Change Order:<br>\$ <u>328,314.80</u>                                     |
| Contract Price with all approved Change Orders:<br>\$ <u>1,436,338.75</u>                                 |

| CHANGE IN CONTRACT TIMES:  |
|--|
| Original Contract Times:<br>Substantial Completion: <u>11/08/24</u><br>Ready for final payment: <u>11/08/24</u><br>(days or dates)                         |
| Net change from previous Change Orders No. <u>1</u> to No. <u>2</u> :<br>Substantial Completion: <u>35</u><br>Ready for final payment: <u>35</u><br>(days) |
| Contract Times prior to this Change Order:<br>Substantial Completion: <u>12/12/2024</u><br>Ready for final payment: <u>12/12/2024</u><br>(days or dates)   |
| Net increase (decrease) this Change Order:<br>Substantial Completion: <u>60</u><br>Ready for final payment: <u>02/10/24</u><br>(days)                      |
| Contract Times with all approved Change Orders:<br>Substantial Completion: <u>02/10/24</u><br>Ready for final payment: <u>02/10/24</u><br>(days or dates)  |

RECOMMENDED:

By: [Signature]

ENGINEER (Authorized Signature)

Date: 12/12/2024

APPROVED:

By: [Signature]

OWNER (Authorized Signature)

Date: 11/7/25

ACCEPTED:

By: [Signature]

CONTRACTOR (Authorized Signature)

Date: 12/12/24

| ITEM NO.                    | DESCRIPTION OF WORK                              | Units | QTY     | Unit Prices                    | SCHEDULED VALUE        |
|-----------------------------|--|-------|---------|--------------------------------|------------------------|
| <b>Original Contract</b>    |  |       |         |                                |                        |
| 10                          | TEMPORARY CONSTRUCTION SIGNS AND BARRICADES      | LS    | 1       | \$ 5,000.00                    | \$ 5,000.00            |
| 20                          | REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT     | SY    | 5002    | \$ 17.00                       | \$ 85,034.00           |
| 30                          | PORTLAND CEMENT CONCRETE PAVEMENT                | SY    | 5002    | \$ 96.50                       | \$ 482,693.00          |
| 40                          | CLASS II BASE COURSE (6" THICK)                  | SY    | 5777.6  | \$ 16.00                       | \$ 92,441.60           |
| 50                          | REMOVAL OF CURBS (CONCRETE)                      | LF    | 3490    | \$ 0.30                        | \$ 1,047.00            |
| 60                          | CONCRETE CURB                                    | LF    | 3490    | \$ 16.00                       | \$ 55,840.00           |
| 70                          | SAW CUTTING                                      | INLF  | 22600   | \$ 0.10                        | \$ 2,260.00            |
| 80                          | RELOCATION OF INFRASTRUCTURE ITEMS               | LS    | 1       | \$ 5,000.00                    | \$ 5,000.00            |
| 90                          | ADJUSTMENT OF MANHOLES                           | EA    | 5       | \$ 400.00                      | \$ 2,000.00            |
| 91                          | ADJUSTMENT OF DRAINAGE STRUCTURES                | EA    | 12      | \$ 500.00                      | \$ 6,000.00            |
| 100                         | EXCAVATION                                       | CY    | 1429    | \$ 21.00                       | \$ 30,009.00           |
| 101                         | EMBANKMENT                                       | CY    | 268     | \$ 50.00                       | \$ 13,400.00           |
| 110                         | SODDING  | SY    | 776     | \$ 20.00                       | \$ 15,520.00           |
| 120                         | REFLECTORIZED RAISED PAVEMENT MARKERS (BLUE)     | EA    | 2       | \$ 350.00                      | \$ 700.00              |
| 140                         | MOBILIZATION / DEMOBILIZATION                    | LS    | 1       | \$ 17,000.00                   | \$ 17,000.00           |
| 10                          | TEMPORARY CONSTRUCTION SIGNS AND BARRICADES      | LS    | 1       | \$ 700.00                      | \$ 700.00              |
| 20                          | REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT     | SY    | 600     | \$ 17.00                       | \$ 10,200.00           |
| 30                          | PORTLAND CEMENT CONCRETE PAVEMENT                | CY    | 600     | \$ 96.50                       | \$ 57,900.00           |
| 40                          | CLASS II BASE COURSE (6" THICK)                  | SY    | 706.7   | \$ 16.00                       | \$ 11,307.20           |
| 50                          | REMOVAL OF CURBS (CONCRETE)                      | LF    | 480     | \$ 0.30                        | \$ 144.00              |
| 60                          | CONCRETE CURB                                    | LF    | 480     | \$ 16.00                       | \$ 7,680.00            |
| 70                          | SAW CUTTING                                      | INLF  | 3163    | \$ 0.10                        | \$ 316.30              |
| 100                         | EXCAVATION                                       | CY    | 181     | \$ 21.00                       | \$ 3,801.00            |
| 101                         | EMBANKMENT                                       | CY    | 36      | \$ 50.00                       | \$ 1,800.00            |
| 110                         | SODDING  | SY    | 107     | \$ 20.00                       | \$ 2,140.00            |
| 130                         | PLASTIC REFLECTIVE PAVEMENT STRIPING (24" WIDTH) | LF    | 24      | \$ 90.00                       | \$ 2,160.00            |
| 140                         | MOBILIZATION / DEMOBILIZATION                    | LS    | 1       | \$ 2,500.00                    | \$ 2,500.00            |
|                             |  |       |         | <b>Original Contract Total</b> | <b>\$ 914,593.10</b>   |
| <b>Plan Change Order 01</b> |  |       |         |                                |                        |
| Field Change 01             | DRAINAGE PIPE REPLACEMENT                        | LF    | 55      | \$ 209.25                      | \$ 11,508.75           |
| Field Change 02             | DRIVEWAY REPLACEMENT                             | SY    | 100     | \$ 124.97                      | \$ 12,497.00           |
| Field Change 03             | GRADE AND COMPACT EXISTING BASE                  | SY    | 3800    | \$ 2.51                        | \$ 9,538.00            |
| 20                          | REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT     | SY    | 1296.85 | \$ 17.00                       | \$ 22,046.45           |
| 30                          | PORTLAND CEMENT CONCRETE PAVEMENT                | SY    | 1296.85 | \$ 96.50                       | \$ 125,146.03          |
| 50                          | REMOVAL OF CURBS (CONCRETE)                      | LF    | 747.2   | \$ 0.30                        | \$ 224.16              |
| 60                          | CONCRETE CURB                                    | LF    | 747.2   | \$ 16.00                       | \$ 11,955.20           |
| 70                          | SAW CUTTING                                      | INLF  | 5152.6  | \$ 0.10                        | \$ 515.26              |
|                             |  |       |         | <b>Plan Change 01 Total</b>    | <b>\$ 193,430.85</b>   |
|                             |  |       |         | <b>Revised Contract Amount</b> | <b>\$ 1,108,023.95</b> |
| <b>Plan Change Order 02</b> |  |       |         |                                |                        |
| Field Change 01             | 10" PCCP   | LF    | 1734    | \$ 139.20                      | \$ 241,372.80          |
| 20                          | REMOVAL OF PORTLAND CEMENT CONCRETE PAVEMENT     | SY    | 2276    | \$ 17.00                       | \$ 38,692.00           |
| 30                          | PORTLAND CEMENT CONCRETE PAVEMENT                | SY    | 500     | \$ 96.50                       | \$ 48,250.00           |
|                             |  |       |         | <b>Plan Change 02 Total</b>    | <b>\$ 328,314.80</b>   |
|                             |  |       |         | <b>Revised Contract Amount</b> | <b>\$ 1,436,338.75</b> |



Site Preparation • Utilities • Concrete Paving • Trucking • Construction Materials

12/6/24

Tony San Martin  
Digital Engineering  
527 West Esplanade Ave.  
Kenner, LA 70065

**Re: St. Charles Concrete Maintenance 2023-24-PCO#4- 10" PCCP on Aries, Scorpio, Leo**

Mr. San Martin,

On several of the streets included in the base bid, we have been instructed to not excavate and replace the base course, instead leaving the existing base material in-place under the new concrete. Our grading cost under the new concrete paving was included in the install cost of the new base course. By eliminating the base course, we have no mechanism to get compensated for the grading required under the concrete.

Therefore, we are pleased to provide our price for work on the above referenced project. This price would include all labor, materials, equipment, and supervision necessary to complete the following work:

| Item # | Description | QTY    | U/M | Bid Unit  | Total         |
|--------|-------------|--------|-----|-----------|---------------|
| 1      | 10" PCCP    | 1733.3 | SY  | \$ 139.20 | \$ 241,275.36 |

This cost should be added to our contract through a change order. Should you have any questions or if you need any additional information, please do not hesitate to contact me directly at 504-214-3849.

Sincerely,  
Kass Bros., Inc.

Brad Leckert  
Vice President of Operations

P. O. Box 487 • Westwego, Louisiana 70096-0487 • Phone: 504.348.9018 • Fax: 504.340.0339  
700 River Road • Westwego, Louisiana 70094  
Website: [www.kassbros.com](http://www.kassbros.com)

Approved LADOTD DBE / SBE Contractor / LA Contractor's License #14914

# KASS

## CONSTRUCTION

### COST BREAKDOWN

Description: 10" PCCP Install- 200 sy set up per day, 300 sy pour per day  
 Quantity: 1733.3 SY

| Labor                              | Unit | Qty | Hourly Rate | Total               |
|------------------------------------|------|-----|-------------|---------------------|
| Foreman                            | HRS  | 145 | \$ 45.50    | \$ 6,597.50         |
| Operator -1 ea                     | HRS  | 145 | \$ 36.00    | \$ 5,220.00         |
| Carpenter /Finisher -4 EA          | HRS  | 580 | \$ 34.00    | \$ 19,720.00        |
| Laborer -4 EA                      | HRS  | 580 | \$ 23.00    | \$ 13,340.00        |
| <b>Labor Cost</b>                  |      |     |             | <b>\$ 44,877.50</b> |
| 33% Field Supervision Cost         |      |     |             | \$ 14,809.58        |
| <b>Subtotal</b>                    |      |     |             | <b>\$ 59,687.08</b> |
| Public Liability Insurance (7.86%) |      |     |             | \$ 3,527.37         |
| FICA/FUTA/SUTA (10.68%)            |      |     |             | \$ 4,792.92         |
| Workers Comp (6.80%)               |      |     |             | \$ 3,051.67         |
| <b>Insurance &amp; Taxes Cost</b>  |      |     |             | <b>\$ 11,371.96</b> |

**TOTAL LABOR**

**\$71,059.03**

| Materials                     | Quantity | Unit  | Unit Price plus tax | Total                |
|-------------------------------|----------|-------|---------------------|----------------------|
| Concrete -4000 psi-3 Day      | 530.00   | CY    | \$182.05            | \$ 96,486.50         |
| Concrete Fuel Surcharge       | 53.00    | EA    | \$38.50             | \$ 2,040.50          |
| Highway Mesh                  | 124.00   | SHEET | \$77.28             | \$ 9,582.72          |
| Form Boards 2" x10" x 12'     | 100.00   | EA    | \$15.52             | \$ 1,551.62          |
| Curing Compound               | 156.00   | gal   | \$5.21              | \$ 812.76            |
| Keyway with #4 Bar and Stakes | 1200.00  | lf    | \$2.85              | \$ 3,420.00          |
| Contraction Joints Baskets    | 1040.00  | lf    | \$9.79              | \$ 10,181.60         |
| Expansion Joint Redwood       | 68.00    | LF    | \$5.61              | \$ 381.48            |
| Grease                        | 9.00     | gal   | \$36.21             | \$ 325.89            |
| Smooth Dowels 1 1/2"          | 150.00   | ea    | \$7.22              | \$ 1,082.40          |
| Epoxy                         | 27.00    | ea    | \$22.00             | \$ 594.00            |
| #4 Bars -2 Ft                 | 55.00    | EA    | \$1.04              | \$ 57.20             |
| Silicone Joint Sealant        | 75.00    | ea    | \$16.46             | \$ 1,234.50          |
| <b>Material Cost</b>          |          |       |                     | <b>\$ 127,751.17</b> |

**TOTAL MATERIAL**

**\$127,751.17**

| Equipment                 | Amt | HR     | HR Rate | Total               |
|---------------------------|-----|--------|---------|---------------------|
| Skid Steer                | 1   | 145.00 | \$51.80 | \$ 7,511.00         |
| Service Truck             | 1   | 145.00 | \$24.00 | \$ 3,480.00         |
| <b>Equipment Subtotal</b> |     |        |         | <b>\$ 10,991.00</b> |

**TOTAL EQUIPMENT**

**\$10,991.00**

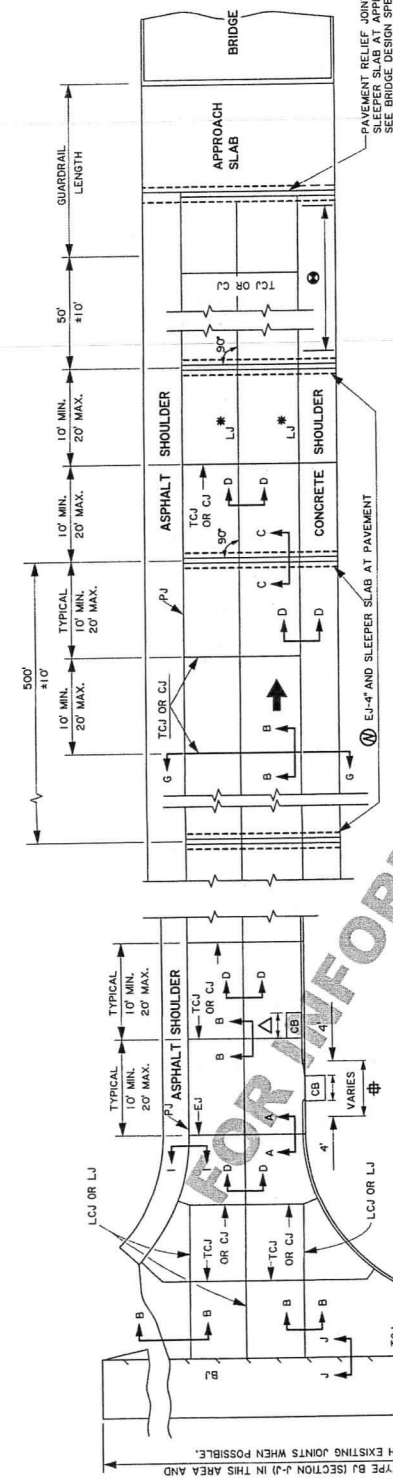
|                          |                     |
|--------------------------|---------------------|
| <b>Subtotal</b>          | <b>\$209,801.20</b> |
| <b>15% Profit Markup</b> | <b>\$31,470.18</b>  |
| <b>Grand Total</b>       | <b>\$241,271.38</b> |
| <b>Unit Price-Per SY</b> | <b>\$139.20</b>     |



**JOINT ABBREVIATIONS:**

- LJ - LONGITUDINAL JOINT
- EJ - TRANSVERSE EXPANSION JOINT
- TCJ - TRANSVERSE CONTRACTION JOINT
- CJ - CONTRACTION JOINT
- LCJ - LONGITUDINAL CONTRACTION JOINT
- LBJ - LONGITUDINAL BUTT JOINT
- BJ - TRANSVERSE BUTT JOINT
- PJ - PAVEMENT EDGE SEAL JOINT

**PLAN VIEW  
 ROADWAY SHOWING JOINTS**



**TABLE 1 - SCHEDULE OF DIMENSIONS**  
 (ALL DIMENSIONS ARE IN INCHES)

| SLAB THICKNESS | SMOOTH DOWEL SIZE (DIA.) | DEFORMED TIE BARS | LENGTH (IN) | SPACING (IN) | KEYWAY |       |
|----------------|--------------------------|-------------------|-------------|--------------|--------|-------|
|                |                          |                   |             |              | A      | B     |
| 8              | 1 1/4                    | 18                | 12          | 1/2          | 24     | 2 1/2 |
| 9              | 1 1/4                    | 18                | 12          | 1/2          | 24     | 2 1/2 |
| 10             | 1 1/2                    | 18                | 12          | 1/2          | 24     | 2 1/2 |
| 11             | 1 1/2                    | 18                | 12          | 3/4          | 30     | 2 1/2 |
| 12             | 1 1/2                    | 18                | 12          | 3/4          | 30     | 2 1/2 |
| 13             | 1 1/2                    | 18                | 12          | 3/4          | 30     | 2 1/2 |
| 14             | 1 1/2                    | 18                | 12          | 3/4          | 30     | 2 1/2 |

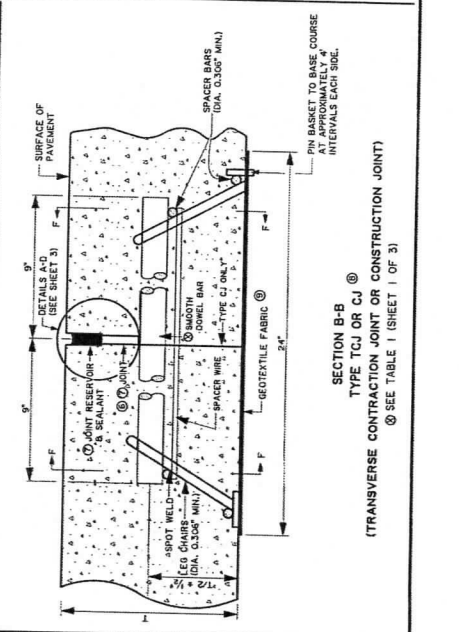
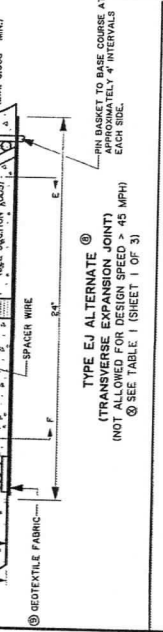
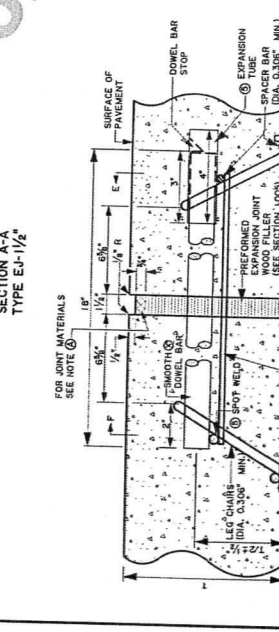
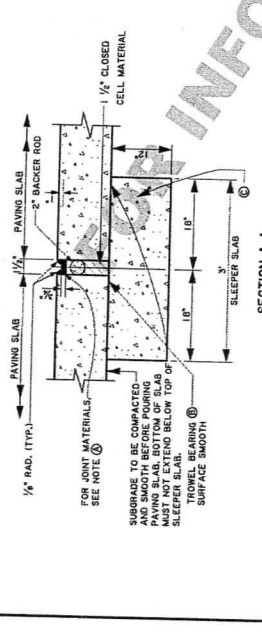
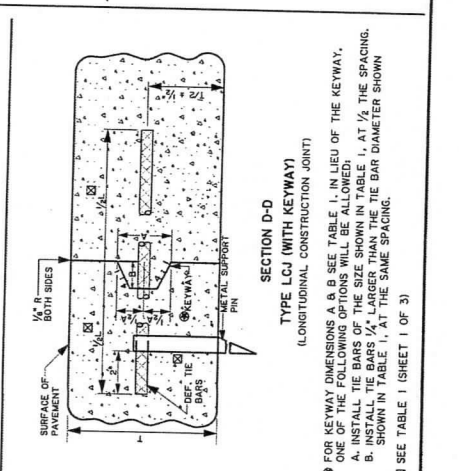
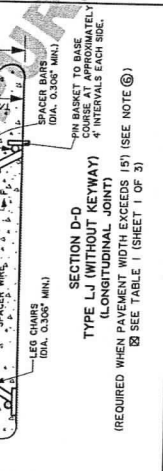
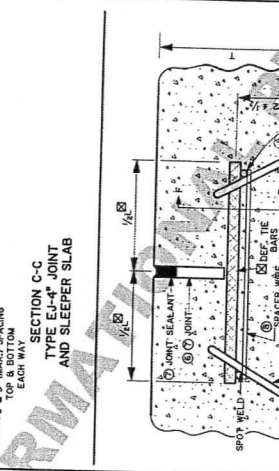
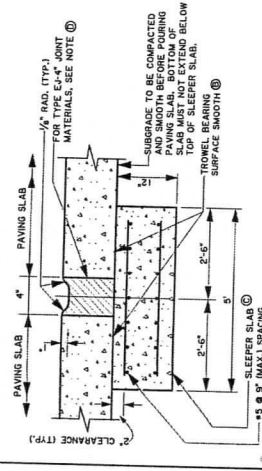
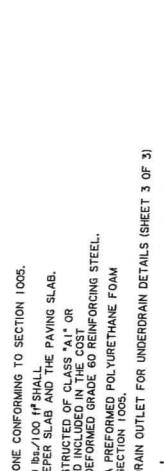
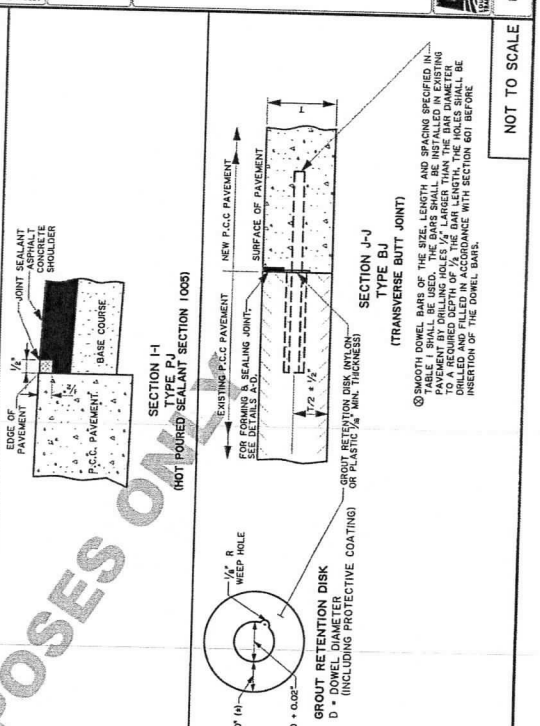
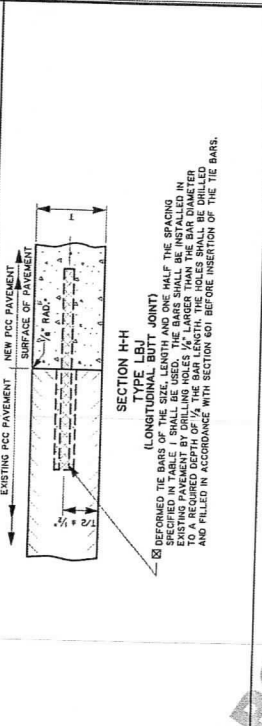
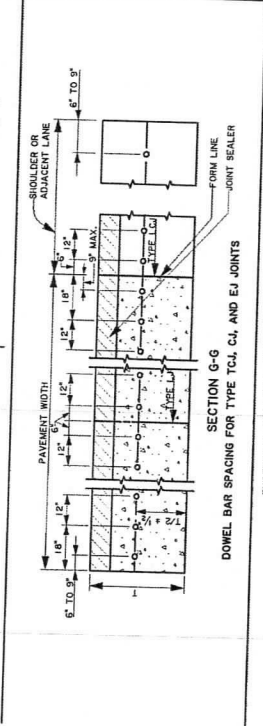
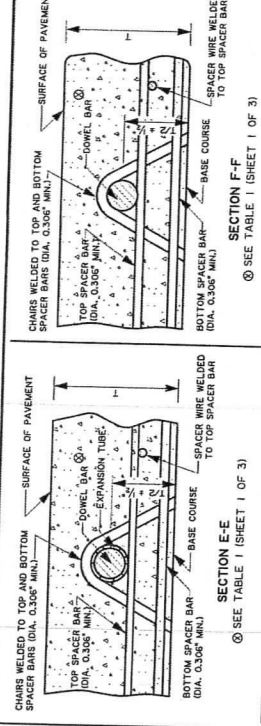
NOTE: MAXIMUM JOINT SPACING AT 18' WHEN PAVEMENT IS PLACED ON PERMEABLE BASE. (SEE SECTION 307)

**NOTES:**

- 1) PAVEMENT EDGES SHALL BE SLIGHTLY ROUNDED (1/4" APPROX.).
- 2) ASPHALT CONCRETE SHOULDER. THE SHOULDER JOINTS SHALL BE SAW CUT AND CONSTRUCTED IN ACCORDANCE WITH SECTION H (SHEET 2 OF 3).
- 3) FOR SECTIONS A-A THROUGH J-J (SEE SHEET 2 OF 3).
- 4) ALL JOINTS TO BE USED WHERE SHOWN ON THIS SHEET OR AS SHOWN ELSEWHERE IN THE PLANS OR AS OTHERWISE DIRECTED BY THE ENGINEER.
- 5) ON TYPE EJ ALTERNATE JOINTS, SPOT WELD ALTERNATE ENDS OF DOWEL BARS TO DOWEL BASKETS AND PLACE EXPANSION TUBES ON FREE ENDS OF DOWEL BARS.
- 6) FOR DESIGN SPEEDS GREATER THAN 45MPH, AND CJ JOINTS AS IN SAW CUT AND CONSTRUCTED WITH A REMOVABLE FORMING DEVICE AS SPECIFIED IN DETAIL "C" (SHEET 3 OF 3), THOROUGHLY CLEAN THE JOINT FACES BY SANDBLASTING FOLLOWED BY AN OIL-FREE AIR JET IMMEDIATELY PRIOR TO SEALING WITH A POURED OR EXTRUDED SEALANT CONFORMING TO SECTION 1005.
- 7) FOR DESIGN SPEEDS OF 45MPH OR LESS.
  - A. SAW CUT AND SEAL TYPE LJ JOINTS AS DESCRIBED IN NOTE 6.
  - B. CONSTRUCT TYPE TCJ OR CJ JOINTS AS DESCRIBED IN NOTE 6 OR IN DETAIL "C" (SHEET 3 OF 3), THOROUGHLY CLEAN THE JOINT FACES IMMEDIATELY PRIOR TO SEALING WITH A POURED OR EXTRUDED SEALANT CONFORMING TO SECTIONS 601 AND 1005.
  - C. WHEN SEALING JOINTS AS SHOWN IN DETAIL "C" (SHEET 3 OF 3), THE SEALER SHALL CONFORM TO SECTION 1005 AND BE INSTALLED IN ACCORDANCE WITH SECTION 601 AND NO ADDITIONAL SEALANT IS REQUIRED.
- 8) EXCEPT AS NOTED BELOW, DOWEL BARS B, C, OR D UNDER ALL TCJ, CJ, AND EJ ALTERNATE JOINTS WHEN DOWEL BASKETS ARE USED SHALL BE HELD IN PLACE BY SUPPORTS SIMILAR TO THE ONES SHOWN, OR APPROVED EQUIVALENTS. APPROVED MECHANICAL PLACEMENT OF DOWEL BARS AND TIE BARS WILL BE ALLOWED WITH ALL PAVING METHODS.
- 9) INSTALL GEOTEXTILE FABRIC (TYPE B, C, OR D) UNDER ALL TCJ, CJ, AND EJ ALTERNATE JOINTS WHEN DOWEL BASKETS ARE USED. THE GEOTEXTILE FABRIC SHALL BE ANCHORED TO THE BASE COURSE WITH PINS. THE GEOTEXTILE FABRIC SHALL BE ANCHORED TO THE BASE COURSE WITH PINS.
- 10) WHEN CONSTRUCTING CONCRETE CURB AND GUTTER ADJACENT TO NEW P.C.C. PAVEMENT, USE TYPE LCJ JOINT WHEN ADJACENT TO EXISTING P.C.C. PAVEMENT, USE TYPE LBJ JOINT. THE FIRST JOINT TRANSFER DEVICE SHALL BE INSTALLED 18' FROM THE PAVEMENT EDGE.
- 11) TRANSVERSE EXPANSION JOINTS ARE NOT TO BE USED FOR CONSTRUCTION JOINTS.
- 12) CONCRETE SHOULDERS:
  - A. CONSTRUCT TCJ JOINTS IN ACCORDANCE WITH SECTION B-B (SHEET 2 OF 3).
  - B. CONSTRUCT LCJ JOINTS IN ACCORDANCE WITH TYPE LCJ DETAIL. SEE SECTION D-D (SHEET 2 OF 3).
  - C. USE THE MAXIMUM SHOULDER THICKNESS WHEN DETERMINING DOWEL BAR AND TIE BARS SIZES IN TABLE 1.
  - D. WHEN SKEWED JOINTS ARE USED ON MAINLINE PAVING, THE SHOULDER TCJ JOINTS MAY BE SKEWED OR CONSTRUCTED AT 90°.
  - E. SHOULDER JOINTS AND JOINT MATERIALS SHALL MATCH THE MAINLINE.
  - F. HEIGHT OF DOWEL BASKET SHALL BE BASED ON THE THINNESS OF THE PAVEMENT. THE HEIGHT OF THE DOWEL BASKET SHALL BE ALLOWED TO KEEP THE DOWEL BAR LOCATED WITHIN TOLERANCE.
- 13) TIE BARS SHALL NOT BE PLACED WITHIN 18' OF CONTRACTION OR EXPANSION JOINTS.

FOR INFORMATIONAL PURPOSES ONLY





**NOTES:**  
 A. ONE OR TWO COMPONENT SILICONE CONFORMING TO SECTION 1005.  
 B. TAR PAPER EQUIVALENT TO 30 lbs./100 sq. ft. SHALL BE PLACED BETWEEN THE SLEEPER SLAB AND THE PAVING SLAB.  
 C. SLEEPER SLAB SHALL BE CONSTRUCTED OF CLASS "A1" OR PAVEMENT TYPE CONCRETE AND INCLUDED IN THE COST OF THE PAVEMENT. PROVIDE DEFORMED GRADE 60 REINFORCING STEEL.  
 D. JOINT FILLER SHALL BE FILLED WITH A PREFORMED POLYURETHANE FOAM FILLER CONFORMING TO SECTION 1005.  
 E. SEE DETAIL "G" - EJ-4 BASE DRAIN OUTLET FOR UNDERDRAIN DETAILS (SHEET 3 OF 3).

SURFACE TO BE COMPACTED AND SMOOTH BEFORE PAVING SLAB. BOTTOM OF SLEEPER SHALL EXTEND BELOW TOP OF SLEEPER SLAB. TROWEL BEARING SURFACE SMOOTH.

FOR JOINT MATERIALS SEE NOTE (A).  
 PIN BASKET TO BASE COURSE AT APPROXIMATELY 4" INTERVALS EACH SIDE.

FOR KEYWAY DIMENSIONS A & B SEE TABLE 1, IN LIEU OF THE KEYWAY. OPTIONS WILL BE ALLOWED.  
 A. INSTALL TIE BARS 1/2" LARGER THAN THOSE SHOWN IN TABLE 1, AT 1/2" THE SPACING.  
 B. SHOWN IN TABLE 1, AT THE SAME SPACING.

SURFACE TO BE COMPACTED AND SMOOTH BEFORE PAVING SLAB. BOTTOM OF SLEEPER SHALL EXTEND BELOW TOP OF SLEEPER SLAB. TROWEL BEARING SURFACE SMOOTH.



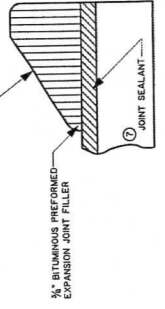
DATE: 10/13/2021  
 APPROVED BY: CHIEF ENGINEER

|         |          |
|---------|----------|
| SECTION | T. LAM   |
| SECTION | D. SMITH |
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NOT TO SCALE

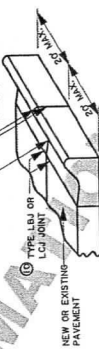
NOTE: SEE STANDARD PLAN DW-01 AND PLANS FOR CURB PLACEMENT DETAILS.

NOTE: WHEN CURB IS PAVERED MANUALLY WITH PAVEMENT, JOINTS ARE CONSTRUCTED BY SAWING THROUGH CURB AND UNDERLYING PAVEMENT. THE SUBSEQUENT WIDENING CUT FOR THE JOINT SEALANT RESERVOIR SHALL EXTEND INTO THE CURB FOR A DEPTH OF 3" (APPROX.) AND INTO THE PAVEMENT TO A DEPTH OF 3" (APPROX.). ALL CURB JOINTS SHALL BE MAINTAINED AT THE GUTTER LINE. ALL CURB TRANSVERSE JOINTS IS SAWED THROUGH CURB.

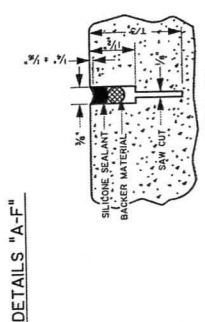


JOINT FILLER DETAIL FOR INTEGRAL CONCRETE CURB (BARRIER OR MOUNTABLE TYPE)

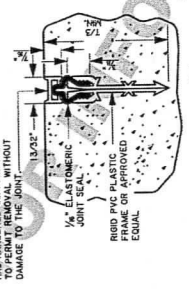
WHEN CURB-FORM SYSTEM IS USED, THIS FACE TO BE TROWEL CUT TO A DEPTH OF 3" (APPROX.), THEN SCORED AT MAXIMUM INTERVALS OR TO MATCH ROADWAY JOINTS.



DETAIL SHOWING JOINTS IN CONCRETE CURB AND GUTTER (EXTEND ALL TCJ THROUGH CURB & GUTTER)



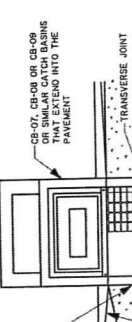
DETAIL "A" USE THIS DETAIL IN CONSTRUCTION WITH TYPE TCJ (SECTION B-B) AND TYPE TCJ JOINT (SECTION D-D) AND NOTES @ B & D ON SHEET "A".



DETAIL "B"

REINFORCERS AFTER CONCRETE HAS HARDENED SUFFICIENTLY TO PERMIT REMOVAL WITHOUT DAMAGE TO THE JOINT.

1/2" ELASTOMERIC JOINT SEALANT TO BE PLACED IN FRAME OR APPROVED FRAME OF EQUAL RADIUS.

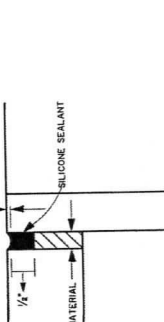


DETAIL "D"

NOT ALLOWED WHEN THE PAVEMENT IS PLACED ON PERMEABLE BASES.

NOT ALLOWED FOR DESIGN SPEEDS GREATER THAN 40 MPH.

NOT ALLOWED FOR DESIGN SPEEDS GREATER THAN 45 MPH.



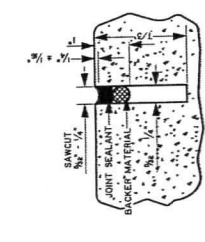
DETAIL "E"

1/2" EXPANSION JOINT FILLER (SEE DETAIL "F")

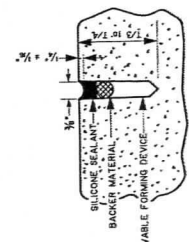
BACK OF CURB

FACE OF CURB

REFER TO TABLE



DETAIL "F"



DETAIL "G"

AFTER CATCH BASIN TOP IS POURED, THE TOP OF THE 3/4" JOINT FILLER IS TO BE REMOVED TO THE DEPTH SHOWN PRIOR TO SEALING. THE CURB FACES ADJACENT TO THE BASIN SHALL ALSO BE SEALED. JOINT FACES SHALL BE CLEANED IN ACCORDANCE WITH SECTION 601.



DETAIL "H"



DETAIL "I"

LENGTH TO BE COMPUTED ALONG THIS LINE

LENGTH TO BE COMPUTED ALONG THIS LINE



BAR DETAIL

SHOWING DIMENSIONS AND SPACING OF 4" CURB BARS AND LONGITUDINAL BARS FOR CONC. CURB

CURB BARS SHALL BE PLACED AT CONTRACTION OR EXPANSION JOINTS.

GAP BAR AT CONTRACTION JOINTS

2" FOR COMBINATION CURB & GUTTER (MOUNTABLE TYPE)

2" FOR COMBINATION CURB & GUTTER (BARRIER TYPE)

4" x 4" BARS ON 24" CTRS. (SEE BAR DETAIL THIS SHEET)

4" x 4" BARS ON 24" CTRS. (SEE BAR DETAIL THIS SHEET)

4" x 4" BARS ON 24" CTRS. (SEE BAR DETAIL THIS SHEET)

4" x 4" BARS ON 24" CTRS. (SEE BAR DETAIL THIS SHEET)

4" x 4" BARS ON 24" CTRS. (SEE BAR DETAIL THIS SHEET)



SECTION K-K



SECTION L-L (WITH CONCRETE SHOULDER)

\* SEE TYPICAL SECTION FOR DEPTH (12" MIN)

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

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EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT



SECTION M-M



SECTION N-N

(SLEEPER SLAB NOT SHOWN)

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

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EMPAVEMENT OR NON-PLASTIC EMBANKMENT

EMPAVEMENT OR NON-PLASTIC EMBANKMENT

St Charles Parish Industrial Rd

Aries Scorpi intersection

| Location     | Lane  | Length Existing: LF | Width Existing: LF | Length New: LF | Width New: LF | Removal: SY | Pour: SY |
|--------------|-------|---------------------|--------------------|----------------|---------------|-------------|----------|
| Aries 1      | Right | 76                  | 12                 | 76             | 12            | 101.3       | 101.3    |
| Aries 2      | Left  | 76                  | 12                 | 76             | 12            | 101.3       | 101.3    |
| Scorpio 1    | Right | 21                  | 12                 | 21             | 12            | 28.0        | 28.0     |
| Scorpio 2    | Left  | 21                  | 12                 | 21             | 12            | 28.0        | 28.0     |
| Aries Radius | Right | 0                   | 0                  | 44             | 4             | 0.0         | 0.0      |
| Driveway 1   | N/A   | 32                  | 37                 | 32             | 37            | 131.6       | 131.6    |
| Driveway 2   | N/A   | 21                  | 28                 | 21             | 28            | 65.3        | 65.3     |
| Total        |       |                     |                    |                |               | 455.6       | 475.1    |

Scorpio

Panel numbered moving East towards Leo

| Location | Lane  | Length Existing: LF | Width Existing: LF | Length New: LF | Width New: LF | Removal: SY | Pour: SY |
|----------|-------|---------------------|--------------------|----------------|---------------|-------------|----------|
| Panel 1  | Right | 40                  | 12                 | 40             | 12            | 53.3        | 53.3     |
| Panel 2  | Left  | 40                  | 12                 | 40             | 12            | 53.3        | 53.3     |
| Panel 3  | Right | 42                  | 12                 | 42             | 12            | 56.0        | 56.0     |
| Panel 4  | Left  | 62                  | 12                 | 62             | 12            | 82.7        | 82.7     |
| Panel 5  | Right | 61                  | 12                 | 61             | 12            | 81.3        | 81.3     |
| Panel 6  | Right | 101                 | 12                 | 101            | 12            | 134.7       | 134.7    |
| Panel 7  | Left  | 121                 | 12                 | 121            | 12            | 161.3       | 161.3    |
| Panel 8  | Left  | 81                  | 12                 | 81             | 12            | 108.0       | 108.0    |
| Panel 9  | Left  | 20                  | 12                 | 20             | 12            | 26.7        | 26.7     |
| Total    |       |                     |                    |                |               | 757.3       | 757.3    |

Leo Scorpio Intersection

Panels numbered moving East towards Leo

| Location           | Lane  | Length Existing: LF | Width Existing: LF | Length New: LF | Width New: LF | Removal: SY   | Pour: SY      |
|--------------------|-------|---------------------|--------------------|----------------|---------------|---------------|---------------|
| Panel 10           | Right | 195                 | 12                 | 195            | 12            | 260.0         | 260.0         |
| Panel 11           | Left  | 154                 | 12                 | 154            | 12            | 205.3         | 205.3         |
| Leo Radius         | Right | 0                   | 0                  | 64             | 5             | 0.0           | 35.6          |
| Total              |       |                     |                    |                |               | 465.3         | 500.9         |
| <b>Grand Total</b> |       |                     |                    |                |               | <b>1678.2</b> | <b>1733.3</b> |